

Richmond Official Community Plan



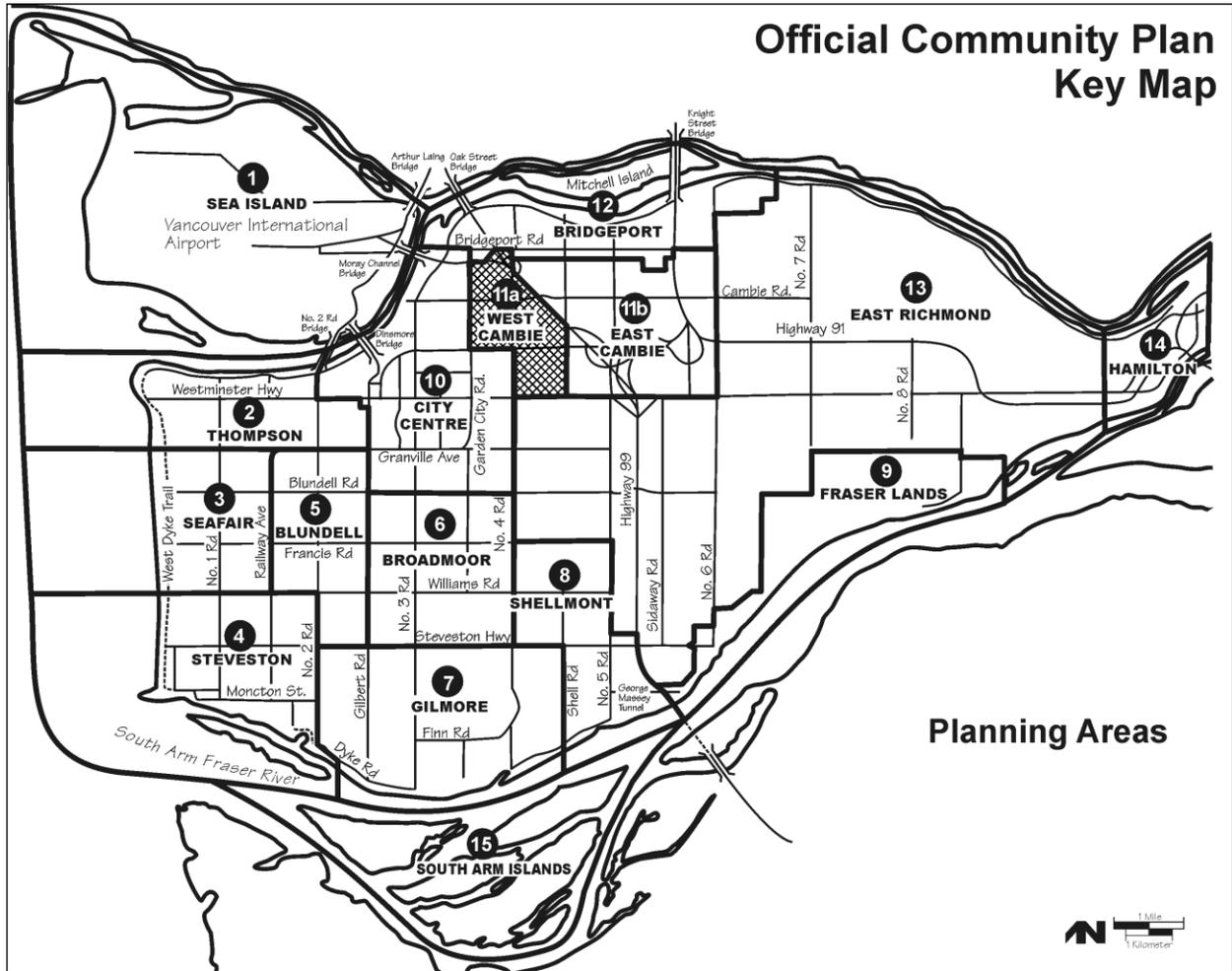
City of Richmond

WEST CAMBIE AREA PLAN Bylaw 7100 Schedule 2.11A

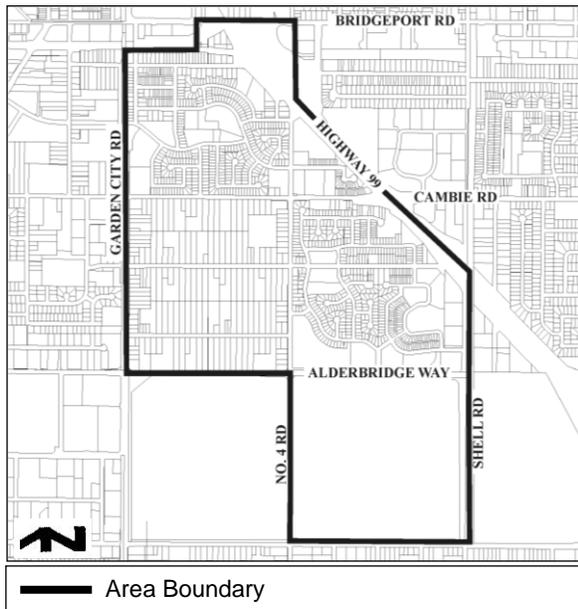


WEST CAMBIE AREA PLAN • WEST CAMBIE AREA PLAN • WEST CAMBIE AREA PLAN

KEY MAP



PLAN AREA MAP



West Cambie Neighbourhood Map



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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

British Columbia's Local Government Act mandates the OCP. It is a legal document for planning and managing the City's social, economic, land use, urban design, servicing and environmental future. It sets out a vision, goals, objectives, policies and guidelines that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps – see inside front cover). Sub-Area plans refer to smaller localized areas within specific planning areas. The OCP addresses broad citywide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map as West Cambie. The plan sets out an overall vision for the area through related goals and planning principles that pertain to land use, urban design, transportation, servicing, environmental and community amenities. It also includes policies, design guidelines and an implementation strategy to assist Council, City staff, land owners and the development community to work towards the plan's successful implementation in the coming years.

A number of City documents were consulted in the preparation of this proposed plan, including the Official Community Plan (OCP), City Centre Area Plan, State of the Environment 2001 Update, Richmond's Suburban History, Richmond's Parks and Trails Plan, and Richmond Industrial Strategy.

1.2 CONTEXT

West Cambie is located in the north central part of Richmond and abuts the east side of the City Centre (see Key Map). The area is affected by the aircraft noise associated with the nearby Vancouver International Airport, a major consideration in land use, site planning and building design. The area already has excellent access to major transportation connections in all directions and is within walking distance of Richmond's City Centre. By 2010, the area will also have links to the Canada Line light rail system.

West Cambie

Approximately two-thirds of the area is developed close to its full potential — the neighbourhoods of The Oaks and Odlinwood. The Oaks was developed as a low-scale residential neighbourhood from the 1970s through the 1990s; Odlinwood was primarily developed during the 1990s and into the first years of the 21st century, at a low-scale but slightly higher density than The Oaks. Both neighbourhoods have excellent access to main transportation routes and local parks.

Alexandra Neighbourhood

Approximately one-third of West Cambie has not been developed to its full potential as an urban neighbourhood and remains on septic sewage disposal. This area is called the Alexandra neighbourhood and is bounded by Cambie Road, Garden City Road, Alderbridge Way and No. 4 Road. The 150+ ac. sub-area retains a subdivision pattern that is predominantly single-family homes on 0.4 ha (1 ac). lots. There are also two large greenhouse operations, a small convenience centre, gas station and Tomsett Elementary School. There are no City parks in the area. An Environmentally Sensitive Area (ESA) is located in the south-east portion of this neighbourhood (see OCP Environmentally Sensitive Areas Attachment 3 to Schedule 1).

DND Lands

The Department of National Defense owns the area bounded by No. 4 Road, Alderbridge Way, Shell Road and Westminster Highway. This area is designated for “Public, Institutional and Open Space” and no changes are envisioned at this time to the current use of this federally-owned property.

1.3 VISION

The overall vision for West Cambie is to:

- retain / enhance the livability of The Oaks and Odlinwood neighbourhoods;
- encourage redevelopment of the Alexandra neighbourhood as a “complete and balanced” community.

1.4 GOALS

1. Designate land uses that are compatible with overall City objectives.
2. Promote opportunities that improve the overall quality of life for residents of West Cambie and support practices that create a sustainable community.
3. Retain existing low-density neighbourhoods and allow for redevelopment for a variety of land uses and densities in the area abutting the City Centre, known as the Alexandra neighbourhood.
4. Recognize West Cambie’s locational advantages for land uses that require good access to the major highway system.

5. Encourage a range of transportation modes that provides access to facilities and services, while minimizing the impacts of traffic, particularly within residential neighbourhoods.
6. Promote and support opportunities for city parks, open space, natural areas, recreation, environmental protection, and heritage preservation, consistent with overall City objectives.

2.0 JOBS & BUSINESS

2.1 ECONOMIC DIVERSIFICATION



Local commercial services

ISSUE:

The neighbourhoods of West Cambie are predominantly residential. A broader mix of land uses, including commercial, office, tourism accommodation and light industry will provide the opportunity for greater employment in West Cambie and result in a more efficient use of the road and transit systems that service this area of Richmond.

OBJECTIVE 1:

Maintain and increase the opportunities for business and employment in West Cambie through commercial, office, tourist accommodation and light industry.

POLICIES:

- a) Retain the existing small commercial centres for local convenience shopping (see Land Use Map);
- b) Retain the commercial/light industrial area and commercial/hotel area in the north-east of West Cambie, in proximity to the highway system (see Land Use Map);
- c) Encourage the development for commercial (stores, services) and offices uses in Alexandra (see Alexandra Neighbourhood Land Use Map);
- d) Ensure that the approval of new vehicle-oriented commercial centres is contingent on the provision of all necessary improvements to services and roads;
- e) Encourage the development of a retail and social destination — Alexandra's High Street — that meets the community focal, gathering place needs of West Cambie residents, as well as those of people from other parts of Richmond and Greater Vancouver.

3.0 NEIGHBOURHOODS & HOUSING

3.1 HOUSING CHOICE



New residential development

ISSUE:

Residential land use policies have been developed in response to the community's desire to ensure that West Cambie remains a viable residential neighbourhood while allowing for the development of Alexandra with medium density housing, including townhouses, apartments and live-work opportunities.

OBJECTIVE 1:

Designate residential areas and provide housing choices for a variety of household types and income groups in West Cambie.

POLICIES:

- a) Allow residential development (see Land Use Map);
- b) Encourage a variety of multi-family residential types (eg. affordable and special needs housing);
- c) Make all multiple-family residential sites subject to the Development Permit Guidelines of the City's Official Community Plan and, in the case of Alexandra, to the Development Permit Guidelines (see Section 8.0).

3.2 ESTABLISHED NEIGHBOURHOODS



Single-family neighbourhood

ISSUE:

The Oaks and Odlinwood neighbourhoods are established areas of detached homes and townhouses.

OBJECTIVE 1:

Preserve the existing established residential neighbourhoods of The Oaks and Odlinwood.

POLICIES:

- a) Retain and enhance the character of these neighbourhoods through single detached, duplex and townhouses;
- b) Ensure that there is a compatible relationship between any infill residential development and existing housing through the regulation of the height, scale and setbacks of infill housing.

3.3 DEVELOPMENT OF ALEXANDRA

ISSUE:

Alexandra is a desirable location for the development of more urban uses at higher densities. The mix of land uses should contribute to a complete and balanced community and make a good transition between the City Centre and neighbouring housing areas.

OBJECTIVE 1:

Encourage transitional development along the northern and eastern edges of the Alexandra neighbourhood that is compatible in scale and urban design with the adjacent Odlinwood and The Oaks neighbourhoods.

POLICIES:

- a) Encourage and facilitate redevelopment for Alexandra (see Alexandra Neighbourhood Land Use Map);
- b) No new single family development is permitted in the Alexandra neighbourhood, due to the OCP Aircraft Noise Sensitive Development Policy;
- c) Establish Alexandra as a Development Permit Area and require development to adhere to the guidelines (see Section 8.0).

4.0 TRANSPORTATION

4.1 TRANSPORTATION NETWORK

ISSUE:

As Richmond grows and the West Cambie area infills and redevelops, there is a need for a comprehensive transportation network that integrates the needs of motorists, pedestrians transit users, goods movement and cyclists. Impacts on residents from increased traffic should be minimized or avoided.

OBJECTIVE 1:

Provide a circulation system for West Cambie that allows for vehicle connectivity within and beyond the area, discourages through-traffic, and maintains a safe and attractive environment for walking and cycling.

POLICIES:

Road Network

- a) Manage vehicle travel through a hierarchical road network to ensure efficient and convenient traffic flow while enhancing neighbourhood livability (see West Cambie Transportation Map);
- b) Provide traffic calming measures to discourage through traffic and speeding and increase safety for pedestrians and cyclists;
- c) Provide distinctive design features within the road network to complement the character of West Cambie's neighbourhoods and to emphasize landmark locations.

Walking Network

- d) Provide for safe and convenient pedestrian movements across arterial roads and along newly-constructed roads to encourage walking as the primary travel choice for short distances;
- e) Provide continuous sidewalks, walkways and pathways to schools, neighbourhood service centres, recreational destinations, bus stops, and other high-volume pedestrian areas, including Alexandra's "High Street" in the south-west corner of Alexandra.



Bike and pedestrian path

Cycling Network

- f) Promote cycling as an appealing and environmentally friendly travel choice by increasing cycling opportunities through off-street pathways and along roadways (see Alexandra Neighbourhood Open Space System Map);
- g) Encourage neighbourhood cycling connections to the City's cycling network and major transit systems.

Parking, Loading and Access

- h) Ensure that all commercial, office and industrial uses have adequate parking, traffic circulation, loading and access routes;
- i) Access points along major arterial roads are restricted to intersections with public roads. Direct access to development would be through local or collector roads;
- j) Consider reduced parking requirements for new developments, for developments sharing parking facilities for mixed-use purposes and developments utilizing and encouraging other transportation demand management measures.

4.2 OTHER TRANSPORTATION MEASURES

ISSUE:

Good traffic management is an essential component of any plan (see Section 4.1). However, there is a need for additional transportation-related measures in West Cambie to help reduce the amount of single-occupant vehicle traffic within West Cambie.

OBJECTIVE 1:

Encourage alternatives to single-occupant vehicle use in West Cambie.

POLICIES:

- a) Require a Transportation Demand Management (TDM) Plan for all commercial developments over 929 m² (10,000 ft²) with a view to reducing reliance on private vehicles, particularly during peak traffic periods. The TDM Plan, secured by covenant on title, will include specific measures to promote walking, cycling, and transit use by employees and patrons, and carpooling by employees;

- b) Ensure that sufficient provision is made for bus stops and shelters. Design teams should work with TransLink in the site planning process. Bus stops have the potential to be centres of activity/sociability along roads and adequate space needs to be dedicated for stops, shelters, signage and related furniture.

4.3 ALEXANDRA

ISSUE:

Alexandra is planned to be a more compact and hence, more walkable neighbourhood than many of Richmond’s existing neighbourhoods. The development of Alexandra provides the opportunity to consider certain aspects of the City’s requirements for road corridors and parking strategies.

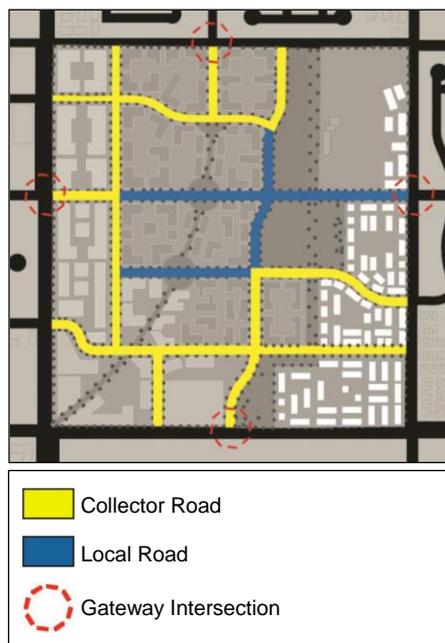
OBJECTIVE 1:

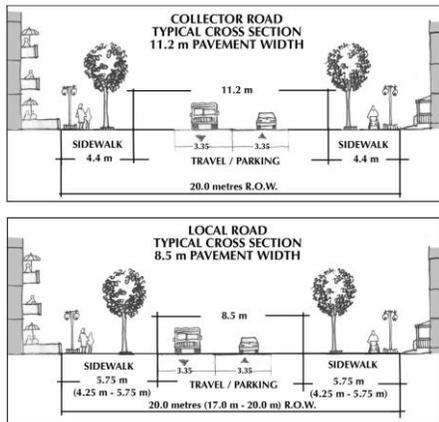
For the Alexandra neighbourhood consider enhanced City standards (road right-of-ways, cross-sections, parking requirements), and include amenities in the public right-of-ways, and by reducing the amount and impact of roads and parking, thus enhancing neighbourhood livability.

POLICIES:

- a) Subject to review by City’s Transportation Department, consider changes to the road cross-sections (see Alexandra Neighbourhood Road System Map) and on-site parking and loading requirements for Alexandra;
- b) Design Alexandra’s “High Street” to incorporate traffic calming measures, curb parking to encourage short-term stays (i.e. high turnover), and civic beautification elements such as public art, special boulevard trees and other decorative features;
- c) In addition to the public road network, give careful consideration to the design of the internal circulation system within all developments to ensure that it responds to the needs, safety and enjoyment of all users. This is merited because of the anticipated large size of land assemblies and developments;
- d) Parking will be primarily enclosed; preferably underground. Where surface parking lots occur, ensure that they are limited in number, size and meet a very high urban design and landscape standard;

Alexandra Neighbourhood Road System Map *Bylaw 8945
2012/10/15*





- e) Wherever possible, in particular for commercial development, entrances to below and above ground parking will be from the internal circulation system of larger projects, rather than from public roads;
- f) Parking entrances should be as small as possible without compromising pedestrian or motorist safety, whether part of an internal circulation system or accessing a public road;
- g) On-site loading is required for all developments. For smaller scale residential developments, on-street loading may be considered.

ISSUE:

Alexandra will require a new network of roads to provide access from the existing surrounding road network to the new development parcels.

OBJECTIVE 2:

Ensure that new roads are provided in a complete and timely manner.

POLICIES:

- a) Development on sites is not to proceed until functional roads are established from the arterial roads connecting to the subject site;
- b) Exact alignment of roadways is subject to detailed functional design, at the sole cost of the developer;
- c) In order to minimize conflicts with bikeways and arterial traffic movement, no private access or driveways would be supported onto the perimeter arterial roads (i.e. Garden City Road, No. 4 Road, Alderbridge Way, and Cambie Road).

5.0 NATURAL & HUMAN ENVIRONMENT

5.1 SUSTAINABILITY



Children's play equipment



Odlin Park

ISSUE:

As Richmond grows, there is a need to ensure that development and change occurs in a sustainable way.

OBJECTIVE 1:

Set high standards for development for West Cambie, including means and methods to promote social, economic and environmental sustainability.

POLICIES:

- a) All development will adhere, as much as possible, to the livability and sustainability guidelines (see Section 8.0).

These guidelines address:

- Community and social infrastructure: affordable, rental and special needs housing; public schools; child care; barrier-free access; crime prevention through environmental design; relationship with the open space system, public art, and heritage (See Section 8.3);
 - Building sustainability: LEED standards, energy and water use, storm water management, solid waste management, and construction waste management.
- b) The City reserves the right to ask a development proponent to undertake various impact or cumulative effects studies at their expense, in relation to community livability and sustainability, including the identification of mitigations, if any are warranted. The scope of such a study will vary depending on the nature of the proposal, but may include such items as environmental matters, traffic and noise impacts, micro climatic change, and impact on policing and security.

5.2 NATURAL AND ACTIVE OPEN SPACE

ISSUE:

West Cambie has a need for active and passive recreational space that fulfils several purposes such as providing some visual relief, allowing natural areas to be used for the study of nature, providing appreciation for our heritage, and appreciating the ecological diversity of the area.

OBJECTIVE 1:

Create a system of parks, greenways and community facilities that serve the residents of West Cambie, preserve sensitive environments, and help to retain Richmond's heritage.

POLICIES:

- a) Continue to improve existing parks for use by all age groups;
- b) Expand existing school/park sites where reasonable to do so;
- c) Ensure that drainage improvements to the area surrounding the Richmond Nature Park do not adversely affect the water levels of the park;
- d) Prepare a detailed park plan for the Alexandra neighbourhood;
- e) Encourage the preservation of the redwood trees at Cambie Road and No. 4 Road, and trees on Alexandra Road and Odlin Road as heritage landscape features;
- f) Improve pedestrian accesses at mid-block locations;
- g) Retain and protect trees and vegetation as per City policies and guidelines.

Bylaw 8945
2012/10/15



5.3 CONNECTIONS

ISSUE:

The existing developed areas of West Cambie need improved pedestrian / cyclist connections between each other and with Alexandra.

OBJECTIVE 1:

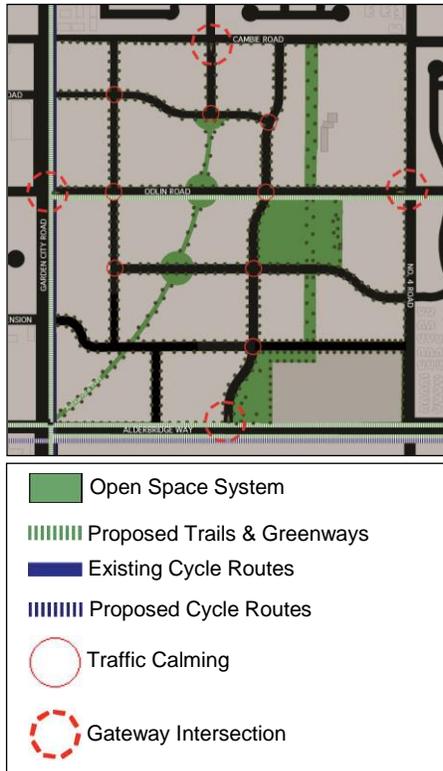
Design and implement pedestrian and cyclist connections within the West Cambie area.

POLICIES:

- a) Design and build a series of green links within West Cambie with safe connections to adjacent areas (see Alexandra Neighbourhood Open Space System Map);
- b) Alexandra Way is a significant feature of this plan that should facilitate walking and cycling throughout West Cambie, not only in Alexandra. Alexandra Way will be privately-owned public-access right-of-way, and include feature privately-owned, publicly-accessible (POPA) open spaces, that links Cambie Road to the Alderbridge/Garden City intersection:

Alexandra Neighbourhood Open Space System Map

Bylaw 8945
2012/10/15



- Alexandra Way is a pedestrian-oriented connection that will be acquired through the development process, in keeping with the guidelines set out below, and maintained through operating agreement between the City and the landowners;
- Alexandra Way will average 13 m (42.7 ft.) in width, with larger areas at street intersections. The exact configuration, location and width will be determined at the time of design development;
- Alexandra Way will accommodate non-motorized traffic of all types and be designed for people with mobility impairments;
- The landscaping treatment will vary over the length of Alexandra Way, corresponding to the variation in land uses and building character. Overall, landscaping will be a mix of soft and hard treatments, including trees, hedges, perennials, annuals, park benches, distinctive paving, places for public art, community gardens and other visual amenities;
- Alexandra Way will be welcoming and safe for users. This requires attention to street lighting, areas for resting and interaction, way-finding signage, and safe pedestrian street crossings;
- Detailed urban design will ensure that Alexandra Way accommodates community needs based on demographics;
- Bicycle/scooter parking, storage, and other end-of-trip facilities will be incorporated at major developments along Alexandra Way.

6.0 COMMUNITY FACILITIES & SERVICES

6.1 COMMUNITY FACILITIES

ISSUE:

A need for an increase in the level of community facilities and services can be expected as the population increases. In most areas of Richmond community services include: education, public safety services such as police and fire protection, leisure services, health care services, childcare, social services, public administration, and religious facilities.

OBJECTIVE 1:

Promote and support opportunities while improving the overall quality of life in the West Cambie planning area.

POLICIES:

- a) Support and, wherever possible, work co-operatively with other agencies and other levels of government to maintain and improve the level of community social services to West Cambie residents;
- b) Continue to encourage the improvement of the educational services to West Cambie residents;
- c) Encourage the expansion of childcare services in West Cambie;
- d) Encourage the development of new space for community services in public and private developments;
- e) For further information see OCP.

7.0 WEST CAMBIE INFRASTRUCTURE

7.1 UPGRADING



New sidewalk and boulevard

ISSUE:

The availability and adequacy of services in parts of West Cambie is poor, especially in Alexandra. Upgrading of services must take place through the development process.

OBJECTIVE 1:

Improve the standard of public utilities (e.g. telephone, natural gas, electric, cable) and services (e.g. water and drainage, sewers) concurrent with population expansion.

POLICIES:

- a) Ensure the provision of efficient utilities necessary for the health and safety of the community;
- b) Upgrade the level of services, such as sanitary sewers, storm sewers, sidewalks, and street lighting in older subdivisions through a variety of mechanisms;
- c) Phase new development to take account of current land use, servicing, and utility constraints;
- d) Provide the necessary improvements to fire protection services concurrent with population expansion;
- e) Utilities will be located underground in road rights-of-way, including hydro, gas, cable, telephone, as well as water and sanitary sewers.

7.2 FLOOD CONSTRUCTION LEVEL

ISSUE:

As part of a broader City Flood Protection and Management Strategy, Flood Construction Levels (FCL) have been reset for the Alexandra neighbourhood. This change is intended to improve the neighbourhood's overall drainage, reduce infrastructure costs, and improve long term flood protection in an area which is anticipated to be largely redeveloped. The increased FCL is designed to coincide, and work in conjunction, with raised road elevations in the area.

OBJECTIVE 1:

Improve drainage and flood protection for the Alexandra neighbourhood.

POLICIES:

- a) Within the Alexandra neighbourhood, Flood Construction Levels (FCL) are established such that the minimum habitable or commercial building floor elevations is 2.6 m Geological Survey of Canada (GSC). Uses such as parking, entrances, foyers, etc. will be permitted below the FCL but may be subject to restrictive covenant requirements;
- b) New roads within the Alexandra neighbourhood will be constructed to a minimum road elevation of 2.0 m GSC, except where lower elevations are required to make the transition to surrounding roads.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 URBAN DESIGN: ALEXANDRA

ISSUE:

Urban Design is the creative design and management of the built urban environment.

The redevelopment of Alexandra provides an uncommon opportunity to set out the City's expectations for urban design, site planning and buildings. Developers and their design teams require guidance on how to achieve the type of community envisaged in this plan.

OBJECTIVE 1:

Foster a distinctive identity for Alexandra through development permit guidelines in private developments and the public realm.

8.1.1 ALEXANDRA DEVELOPMENT PERMIT AREA

The Development Permit Guidelines apply to the five-character areas of Alexandra. The purpose of these guidelines is to supplement the City's comprehensive Development Permit Guidelines set out in the OCP. (Note: The Development Permit Guidelines in the OCP must be consulted by developers and their design teams in addition to the requirements and guidelines set out in this document.)

8.1.2 JUSTIFICATION

It is the intent of these guidelines to support the goals, objectives and policies of the West Cambie Area Plan as they apply to the Alexandra Neighbourhood Land Use Map.

The overall objective of the guidelines is to foster development in the private and public realms that supports an appealing and livable "complete and balanced community". The scope of the proposed redevelopment in Alexandra merits site-by-site consideration of "form and character" in order to achieve this objective, as provided for in the Local Government Act.

8.1.3 OVERALL CHARACTER AND PATTERNS

- Site planning, building design and landscaping will be more urban than suburban in its character and image.
- The open space system will be a major contributor to Alexandra's character, providing a variety of environments for passive and active recreation, walking, and nature enjoyment. Development proposals adjacent to the open space system will demonstrate a careful interface and relationship with this system.
- Alexandra has five distinct character areas related to the various land uses, building forms, and open space encouraged by the Alexandra land use plan. Notwithstanding the distinctiveness of each character area, built form and landscaping throughout Alexandra will be visually complementary and ensure functional connections among the character areas.

8.1.4 LANDSCAPE ELEMENTS

- Landscaping elements for private developments will vary among character areas, primarily in relation to the built form.
- Existing mature/substantial vegetation is limited in Alexandra, owing to its current and former land uses/drainage patterns. However, where this does exist, significant effort should be made to incorporate mature vegetation elements into the new site plan, subject to requirements of new grade elevations to comply with the Floodplain Management Policy.
- In keeping with the aim of a pedestrian-oriented neighbourhood, landscape design will ensure a visual and functional coordination with adjacent parcels and the public realm.
- Privately owned, publicly accessible open spaces (POPAs) are encouraged at highly visible locations, particularly along Alexandra Way. Depending on the adjacent land uses and character area in which these POPAs are created, their design treatment may vary from urban plazas, to play pockets, to tranquility spaces, to community gardens.
- Retaining walls may be appropriate in some character areas. Where these are part of the design treatment, they should be low in height.

- Varied planting design that reflects the diversity of the Fraser River Basin and is suited to Richmond's unique soil conditions will be required; City guidelines on species, installation and maintenance must be adhered to.

8.1.5 BUILDING MASSING AND HEIGHT

Bylaw 9021
2013/10/15



- There will be a gradation of building height from the south-west (Garden City Road and Alderbridge Way) to the north-east of the area — from higher to lower.
- With the exception of a potential hotel within the south-west commercial area, building heights will generally vary between two to four storeys (above one level of parking) — neither single storey nor tower development is appropriate within the residential areas (five and six storeys may be considered for combustible, non-combustible or concrete construction, increased open space, and no additional overshadowing of neighbouring properties).
- The first two storeys above finished grade, of all buildings will be designed in a way that reinforces the outward-looking, pedestrian-oriented character of Alexandra.

8.1.6 ARCHITECTURAL ELEMENTS

- A variety of architectural styles and materials will be encouraged within the overall context of each of the five character areas (see Alexandra Neighbourhood Character Areas Map).
- Care and special attention must be given to architectural elements that have a direct impact on the pedestrian environment. Design details that reinforce the pedestrian-oriented character of Alexandra are particularly encouraged.
- Balconies are encouraged to animate the appearance of a building and encourage a visual/aural relationship with streets and courtyards. Owing to aircraft noise impacts, recessed balconies may be preferred to projecting balconies. Balconies at street corners are particularly encouraged.

Building Exteriors

- A variety of exterior materials is appropriate; preference is for demonstrated complementarity and compatibility with already constructed neighbouring buildings.
- Large expanses of stucco and plain concrete are not appropriate — the use of brick, native stone, glass, decorative concrete, metal siding and wood shingle is preferred.

- Large expanses of one colour are not appropriate — the use of at least three exterior colours is preferred.
- A variety of colours is appropriate, although preference is for demonstrated complementarity and compatibility with already constructed adjacent buildings.
- A warm colour palette is recommended for large expanses of buildings, along with muted and contrasting accent colours. High intensity colours or highly reflective colours are discouraged, except as an accent.

Building Roofs

- Variation in roof lines is appropriate; demonstrated complementarity with already constructed adjacent buildings in shape and colour is encouraged.
- Generally, flat roofs are more acceptable in the western portion of the area, while more domestic roof lines (gable, shed, hip) are appropriate in the eastern portions of the site.
- “Living roofs” should be considered, both as a building amenity and as a community environmental benefit. In some situations an intensive approach is appropriate (user amenity, urban agriculture); in other situations an extensive approach (primarily storm water mitigation) is appropriate.

Building Relationship with Streets

- Buildings will generally be oriented towards the street and public open space rights-of-way with direct pedestrian access from adjacent sidewalks and greenways in order to facilitate pedestrian activity.
- A majority of development in the western half of Alexandra should visually hug the edge of the property line along streets — a build-to line is established within some character areas. Grade-level uses and design features should help to animate the pedestrian environment.
- Side and rear setbacks may vary, depending on the uses and design of the proposed development and neighbouring environments.
- Elevated public walkways/bridges between buildings and across roads are discouraged.
- The employment components of mixed use buildings should be oriented towards the arterial road network (Garden City Road and Cambie Road) to provide a consistent and complementary streetscape with future development on the west side of Garden City Road. Residential and ancillary uses should be inward oriented towards the collector roads (e.g. Dubbert Street).

Bylaw 9121
2015/06/15



Lessening the Impact of Aircraft Noise

*Bylaw 9121
2015/06/15*



- The noise impact of aircraft operations will affect site planning, building design and use of amenity areas, especially within residential complexes located in the north/centre and east/centre of Alexandra.
- There shall be no new lots for single detached housing within the Alexandra Neighbourhood (as identified on the 2041 OCP Aircraft Noise Sensitive Development Map).
- The City's OCP includes a policy designed to lessen the exposure to aircraft noise on the indoor living environment of new housing in specific areas of Richmond, including the Alexandra neighbourhood. This policy will primarily be achieved by extra noise insulation. Additional insulation lessens noise associated with road traffic and aircraft noise that contributes to reducing energy costs.

Developers of all new aircraft noise sensitive land use buildings are required to:

*Bylaw 8561
2010/01/18*



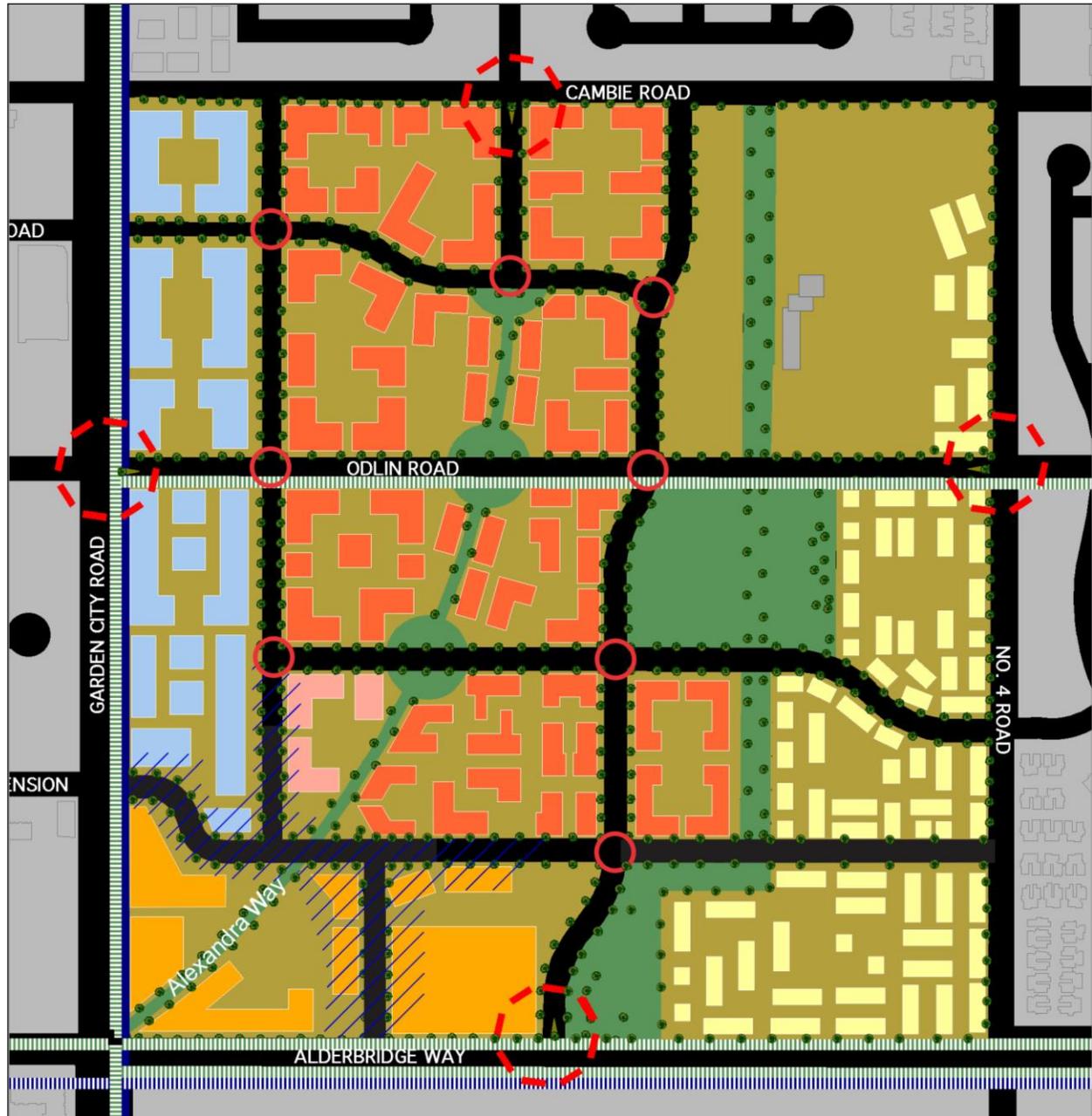
- Sign a Restrictive Covenant agreeing to have the building designed to incorporate adequate sound measures against aircraft noise, before obtaining a rezoning;
- Retain a registered professional who is qualified in acoustics to determine the aircraft noise exposure affecting the property and to determine the measures needed to satisfy Canada Mortgage and Housing Corporation noise insulation standards, prior to submitting an application for a Building Permit;
- Retain a registered professional who is qualified in the design and installation of an air conditioning, or an alternative cooling, system that meets the ASHRAE 55-2004 standard, or subsequent updates as they may occur;
- Retain a registered professional to certify that any required noise insulation measures have been installed according to design, before obtaining final Building Permit approvals.

8.2 ALEXANDRA'S CHARACTER AREAS

Alexandra has five main character areas (see Alexandra Neighbourhood Character Areas Map). These are areas where land uses and densities differ from one another and, therefore, some aspects of their site and building design also differ. The five character areas are described below, along with a series of planning measures/guidelines to be taken into account in the design development process.

Alexandra Neighbourhood Character Areas Map

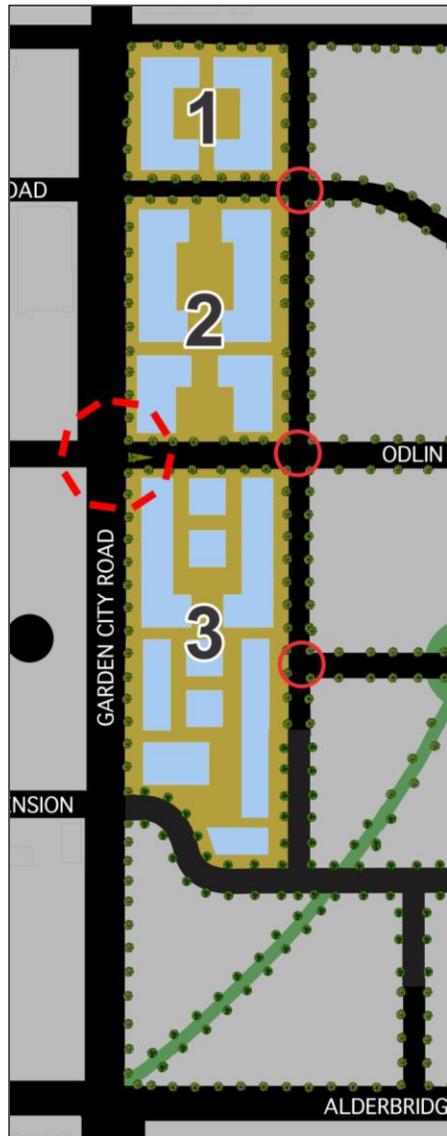
Bylaw 9121
2015/06/15



LEGEND

<p>1 Mixed Use Employment-Residential</p> <p>2 Mixed Use (Retail/Office/Hotel)</p> <p>3 The High Street</p> <p>4 Medium Density Housing</p>	<p>5 Low Density Housing</p> <p>Mixed Use: • a mix of low to medium density residential with low to medium density residential over retail or live/work uses.</p> <p>Open Space System</p>	<p>Proposed Trails & Greenways</p> <p>Existing Cycle Routes</p> <p>Proposed Cycle Routes</p> <p>Traffic Calming</p>	<p>Gateway Intersection</p> <p>The images in this figure illustrate the range of built forms and open spaces that are envisioned for Alexandra. Each of the six character areas is described in the text and illustrated in more detail on Figures 3 through 8.</p>
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**Character Area 1 –
Mixed Use Employment-
Residential Map**



Bylaw 9121
2015/06/15

Bylaw 9121
2015/06/15

**8.2.1 CHARACTER AREA 1 — MIXED USE
EMPLOYMENT-RESIDENTIAL**

This character area is located along the east side of Garden City Road (see Character Area 1 – Mixed Use Employment-Residential Map). The preferred development in this area mirrors the land uses and building scale and building setbacks to the west side of Garden City Road, immediately south of Cambie Road. Multi-family residential uses may be permitted, subject to the applicable provisions of this Area Plan, Section 8.1.6 Architectural Elements – Lessening the Impact of Aircraft Noise, and the 2041 OCP Aircraft Noise Sensitive Development Policy.

Garden City is a high volume arterial road that plays an important role in the City’s transportation system, but to date is not heavily used by pedestrians. As both sides of Garden City gradually redevelop, building forms and edges will help create an improved streetscape for motorists, cyclists and pedestrians.

Development Blocks

Bylaw 9121
2015/06/15

- The development block formed by Cambie Road to the north, Garden City Road to the west, the McKim Way alignment to the south and the Dubbert Street alignment to the east shall be labelled as Block 1.
- The development block formed by the McKim Way alignment to the north, Garden City Road to the west, Odlin Road to the south and the Dubbert Street alignment to the east shall be labelled as Block 2.
- The development block formed by Odlin Road to the north, Garden City Road to the west, Alexandra Road to the south and the Dubbert Street alignment to the east shall be labelled as Block 3.

Minimum Lot Area

- 1.0 ha (2.47 ac).
- No orphan properties of 0.4 ha or less (1 ac. or less), in order to facilitate development as anticipated in this plan and not to perpetuate non-conforming uses (e.g. single detached homes).

Land Uses

- Office commercial.
- Bio-tech, research, and labs.
- Information technology (IT), media/software.
- Institutional (private and public) including medical facilities.
- Employment uses shall comprise minimum of 0.52 FAR of the total floor area within Development Blocks 1, 2 and 3, as identified in the Character Area 1 – Mixed Use Employment-Residential Map.

Bylaw 9121
2015/06/15

- Restaurants and neighbourhood pub.
- Retail and retail services commercial — small floor plate only including service station and neighbourhood commercial, at the southeast corner of Garden City Road and Cambie Road.
- Educational facilities.
- Recreational facilities.
- Enclosed commercial parking, preferably structured.

Floor Area Ratio

*Bylaw 9121
2015/06/15*

- 
- The total building area within each Development Block 1, 2 and 3, excluding underground parking, shall be no greater than a total of 0.52 FAR for employment use (excluding bonus density).
 - The maximum FAR for residential use, based on a minimum of 0.52 FAR of employment space, shall be 1.28 Bonus FAR if a minimum of 5% of the total residential floor space is provided as built Affordable Housing units, with a minimum of an additional 7.5% of residential space being provided in the form of purpose built modest market rental housing units, and a minimum of 2.5% residential floor space as built market rental housing and secured as rental in perpetuity, to meet the City's needs.
 - A minimum of 0.52 FAR of Employment Space shall be completed prior to or concurrent with the completion of the residential space.

Floorplate: Retail

- The maximum retail floor plate is 929 m² (10,000 ft²).

Height

- Two to four storeys (8 m to 15 m) (five storeys (20 m) may be considered for non-combustible or concrete construction, increased open space, and no additional overshadowing of neighbouring properties).
- 
- Minimum of two storeys (8 m) and up to six storeys (25 m) adjacent to the west boundary of each Development Block along Garden City Road.

*Bylaw 9121
2015/06/15*

Frontage

- Limit building frontage to 45 m (147.6 ft.); suggest structural bays between 8 m to 10 m (26.2 ft to 32.8 ft.).



Variety of Architecture

Bylaw 9121
2015/06/15

Bylaw 9121
2015/06/15

Build-to Lines and Setbacks

- The build-to line for the first two storeys should be no more than 3 m (9.8 ft.) behind the property line along Garden City Road and Greenway and the new north-south road, with the exception of building entrances which may be greater. Additional storeys may be setback and do not have a build-to line.
- Build-to lines and setbacks are not established for other property lines and depend on the uses and design in relation to neighbouring buildings.

Site Coverage

- Depends on uses and configuration.

Phasing of Development

- Developments proposing to be completed in one phase shall ensure that the applications include all of the following:
 - a minimum of 0.52 FAR is allocated for employment space;
 - a minimum of 5% of the total residential floor area is provided in the form of built Affordable Housing;
 - a minimum of 7.5% of the total residential floor area is provided in the form of built modest rent controlled rental units;
 - a minimum of 2.5% of the total residential floor area is provided in the form of built market rental units.
- To ensure that the Employment space is built early in any project, all Rezoning, Development Permit and Building Permit applications shall ensure that a minimum of 30% of the floor area (up to 0.52 FAR) for non-residential and employment use is maintained throughout each phase within the applicable Development Block, and that a maximum of 70% of the total built form being provided is for residential floor area (and associated residential accessory uses) until the minimum 0.52 FAR of employment space is provided.
- Subsequent to, or in conjunction with, the employment space being completed, and to ensure that the Affordable Housing, modest rent controlled housing and market rental units are completed as part of the development for the first half of the total residential floor area, for all projects with two or more phases:

Bylaw 9121
2015/06/15



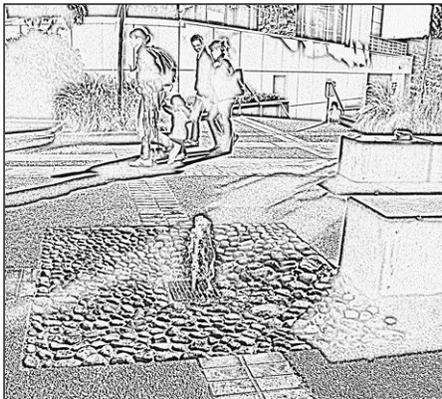
- 15% of the total residential floor area (allocated for Affordable Housing, purpose built modest rent controlled units and market rental units) shall be built and completed as part of the first 50% of the total residential floor area being built within the entire project or Development Block;
- Applications for residential development beyond the first 50% of the total residential floor area may not be processed or considered, unless all of the employment space and affordable housing, modest rental control units and market rental units are completed in their entirety and ready for occupancy.

Parking and Access

- At least 70% of vehicle parking should be set below the building or in structure.
- Vehicle parking should be kept to a minimum that is considered to be viable for the primary use.
- Driveway and vehicle access to underground/structure parking should not be from Garden City Road.
- Shared parking lots are encouraged in order to reduce overall parking.
- No private access and driveways are permitted onto the perimeter arterial roads (Garden City Road and Cambie Road).
- On site loading will be required for all developments, and be screened from the street.

Additional Site, Landscaping Considerations

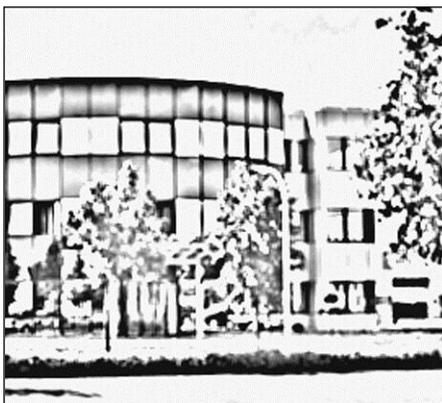
- Linked courtyards at the rear of buildings are suggested as a site amenity. With attention to design/materials, these courtyards may minimize traffic noise, including aircraft noise, in combination with appropriate materials and landscaping.
- There should be a continuous, publicly accessible pedestrian system that visually and functionally connects each development in this area and leads to the High Street. The appropriate width, materials, configuration and landscaping will be considered during design development.
- Sidewalks should be located along all sides of a development. The minimum width, configuration and landscaping will be considered during design development.
- Bus stops and drop-off / pick-up points should be considered during design development; where these are included they should be anchored by special design and landscaping.



Attractive Outdoor Spaces Between Office Buildings



Retail Space at Street Level



Include Boulevards

- The use of mature plant materials will be required in surface parking areas and along vehicle circulation lanes with a view to creating significant landscaping during the early tenure of the developments.

Additional Building Design Considerations

- Building facades should be articulated to provide visual interest consistent with Alexandra's proposed identity.
- Particular attention will be required along the building's frontage(s) — animating design features should be integrated along at least 60% of the facade, such as arcades, windows, entry areas, among others.
- Transparency and translucence are essential at pedestrian eye level.
- Building entrances will orient visitors and be aesthetically pleasing. Buildings should have clearly defined, highly visible entrances.
- Building design, rather than dominant signage and other wayfinding measures, will draw attention to building entrances.
- The inclusion of weather protection measures is encouraged, particularly at building entrances, at key points in the pedestrian system, and at transit stops.
- Signage for site, building and tenants should be expressed in a simple, understated manner. Stand-alone signage is not appropriate.
- Lighting for site, building and tenants should be expressed in a simple, understated manner. Particular attention must be given to lighting for safety/security in pedestrian areas.
- High quality screening of loading areas and utility facilities will be required. Ideally, this should be incorporated into a building's architecture, not only accomplished through landscape treatment.
- Sustainability features of building design may be incorporated in many ways, including: a green roof or any strategy that assists in storm water management, enhanced biomass and energy efficiency; reduction of greenhouse gas (GHG) emissions through energy systems that result in lower greenhouse gas emissions and improved building mechanical systems.
- A flat roof design is acceptable, provided rooftop parking and all mechanical systems are appropriately screened by way of a parapet. Variations in roof line (e.g. mansard, hip, gable) may be acceptable if presented in an authentic manner. Large expanses of reflective and one-material (or colour) roofs are not acceptable.

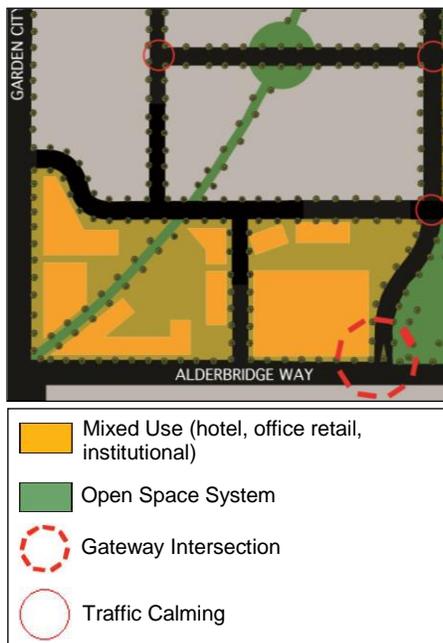
- Outdoor areas for storage, truck parking, garbage collection, and loading are discouraged. Where essential, appropriate screening will be required.
- Stand alone, single-use buildings and/or mixed-use buildings may be considered, provided that they form part of a comprehensive plan for each Development Block, 1, 2 and 3.

Bylaw 9121
2015/06/15



8.2.2 CHARACTER AREA 2 — MIXED-USE (RETAIL-OFFICE-HOTEL)

Character Area 2 – *Bylaw 8945*
Mixed Use Map *2012/10/15*



This character area occupies the south-west area of Alexandra, adjacent to the High Street (see Character Area 2 – Mixed Use Map). This is a very important gateway to Richmond's City Centre and will become a new Richmond landmark. Under no circumstances should design teams consider this character area as being solely "highway-oriented". Development along Alderbridge must be a compact, urban form and meet high standards of site planning and urban design.

There may be several landowners or developers in this area. The City requires that there be a clearly demonstrated unified approach to land planning, landscaping, buildings, signage, and pedestrian systems. Alexandra Way will be a key element of planning in this area.

This area anticipates the development of a portion of the area as large floor plate retail, along with compatible non-residential uses as outlined below. However, all development must demonstrate an appropriate site, building and landscaping response as an integral component of a "complete and balanced" community. Every effort must be made to humanize a proposed large floor plate project.

Minimum Lot Area and Orphan Parcels

- 2 ha (4.94 ac.).
- No orphan properties of 1 ha (2.47 ac.), in order to facilitate development as anticipated in this plan and not to perpetuate non-conforming uses (e.g. single detached homes).

Land Uses

- Hotels, with ancillary amenities.
- Office commercial.
- Restaurants and neighbourhood pub.
- Retail and retail services commercial, including large floor plate retail.
- Educational facilities.
- Recreational facilities.
- Enclosed commercial parking, preferably structured.



Village Architecture

Floor Area Ratio

- See “Alexandra Neighbourhood Land Use Map.”

Floor Plate: Retail

- East of The High Street – no minimum; maximum 9,290 m² (100,000 ft²). Proposals with retail floor plates of more than 9,290 m may be considered, where a high quality urban form is achieved and parking is integrated with the building.
- West of The High Street – no minimum; maximum 2,322 m² (25,000 ft²). Proposals with retail floor plates of more than 2,322 m² may be considered, where they are integrated with the adjacent retail.

Height

- Minimum two storeys (8 m) — all uses; maximum 15 storeys (45 m) — hotel and ancillary uses.

Frontage

- East of the High Street – depends on uses, orientation, location; preference for buildings with frontage less than 90 m (295 ft.).
- West of The High Street – depends on uses, orientation, location; preference for buildings with frontage less than 45 m (147.6 ft.).

Build-to Lines and Setbacks

- The build-to line along Alderbridge and Garden City will be 3 m (9.8 ft.), subject to intersection site line requirements at Alderbridge Way and Garden City Road.
- The build-to line along the primary new road (leading to the High Street) will be 2 m (6.5 ft.).
- The eastern portion of this area is an environmentally sensitive area (ESA) that is identified in the City's OCP. During the design development period special attention will need to be given to the relationship of any building and the ESA. The appropriate setback will be confirmed at the time of a development application.
- Setbacks for other property lines are flexible and depend on the uses and design in relation to neighbouring buildings.



Various Roof Lines

Site Coverage

- Depends on uses and configurations. Up to 60% may be acceptable, depending on site and use amenities.



Public Courtyards



Pavement Variety

Parking and Access

- At least 80% of vehicle parking should be underground or in-structure and not visible from the High Street.
- Surface parking should be configured to mitigate visual impact and the need for landscape screening from the pedestrian system and public roads.
- Permeable asphalt and other storm water percolating treatments should be considered for surface parking areas.
- Vehicle parking should be kept to a minimum that is considered to be viable for the primary use. A maximum number of units will be established for each project proposal. Ideally, the maximum will be less than typically required for large floor plate retail in Richmond, owing to its urban location and excellent transit network — a net reduction of 20% is suggested subject to studies by professional transportation engineers.
- Shared parking lots are encouraged in order to reduce overall parking and to integrate adjacent uses.
- Priority parking should be identified for motorists with disabilities and those who use alternative fuel vehicles, car co-op vehicles, and ride-share vehicles.
- Access points should be consolidated, organized and clearly marked in order to minimize conflict among motorists, pedestrians, and cyclists.
- No private access and driveways are permitted onto the perimeter arterial roads.
- On site loading will be required for all developments.

Additional Site and Landscaping Considerations

Site planning will emphasize pedestrian movement systems over vehicular movement systems.

- There will be a continuous internal pedestrian walkway — Alexandra Way — that visually and functionally connects the focal points of pedestrian activity, such as bus stops, street crossings, building and store/business entrances, feature landscaping. The minimum width, materials, configuration and landscaping must carefully considered during design development.
- Sidewalks should be located along all sides of a building. The minimum width, configuration and landscaping must be carefully considered during design development.

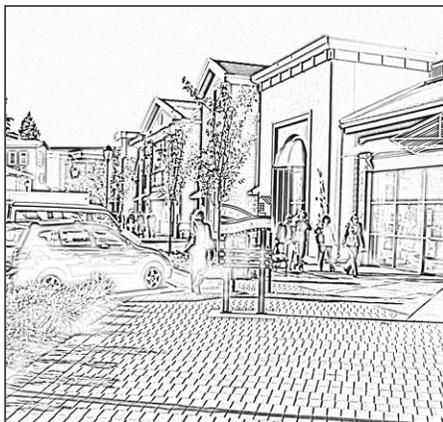


Seating Areas

- Bus stops and drop-off/pick-up points should be considered as integral parts of the pedestrian system and should be anchored by special design and landscaping features.
- Along the new primary road, private developments are encouraged to extend on-site landscaping into the right-of-way where this will help animate the adjacent sidewalks.
- The use of mature plant materials will be required in surface parking areas and along vehicle circulation lanes in order to establish significant landscaping during the early tenure of the developments.
- Storm water drainage requires special attention. The nature park adjacent to the eastern edge of this area may be significantly impacted by drainage changes caused by construction, new buildings, and the ongoing effects of non-permeable surfaces. Design teams are encouraged to explore innovative ways to handle storm water, including potential living roofs, living walls, day lighting and bioswales.

Additional Building Design Considerations

- The perception of massive scale, uniformity and impersonal appearance of large floor plate buildings requires careful design attention.
- Building facades should be articulated to reduce the scale and typical uniform appearance and provide visual interest consistent with Alexandra's proposed identity.
- A "formulaic" or "historical fantasy" design are not appropriate; design teams should strive for authenticity, originality, and complementarity.
- Developments should incorporate wall projections/recesses in an authentic way, avoiding the appearance of being an afterthought.
- Particular attention will be required along the building's frontage(s) — animating design features should be integrated along at least 60% of the facade, such as arcades, display windows, entry areas, expressions of structural verticality, among others.
- Transparency and translucence are essential at pedestrian eye level. Ideally, transparent/translucent windows and doors should occupy 60% of a building's frontage at grade level.
- Building entrances will orient visitors and be aesthetically pleasing. Buildings should have clearly defined, highly visible entrances, incorporating such items as canopies, arcades, feature roof line, wing walls, and integral planters.



Wide Sidewalks



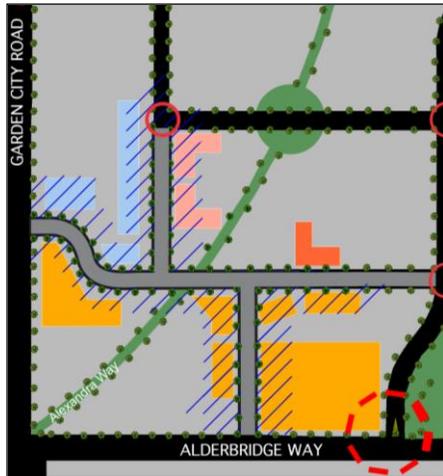
Consistent Street Furniture



Human Scale

- The inclusion of weather protection measures (e.g. awnings, canopies) is encouraged around the pedestrian-oriented faces of buildings, particularly at building entrances, at key points in the pedestrian system, and at transit stops.
- Building design, rather than dominant signage and other wayfinding measures, will draw attention to building entrances. Signage for site, building and tenants should be expressed in a simple, understated manner. Stand-alone signage is not encouraged, and, if required, should be incorporated with feature landscaping.
- Lighting for site, building and tenants should be expressed in a simple, understated manner. Particular attention must be given to lighting for safety/security along pedestrian movement systems.
- High quality screening of loading areas and utility facilities will be required. This should be incorporated into a building's architecture, not only accomplished through landscape treatment.
- Sustainability features of building design may be expressed in many ways, including: a green roof or any strategy that assists in storm water management, enhanced biomass and energy efficiency; reduction of greenhouse gas (GHG) emissions through energy systems that result in lower greenhouse gas emissions and improved building mechanical systems.
- Flat roof design is acceptable, provided rooftop parking and all mechanical systems are appropriately screened by way of a parapet. Variations in roof line (e.g. mansard, hip, gable) may be acceptable if presented in an authentic manner, avoiding the appearance of a design afterthought. Large expanses of reflective and one-material roofs are not acceptable.
- Outdoor areas for storage, truck parking, garbage collection, loading are discouraged. Where not feasible, they should not be visible from public roads, Alexandra Way or other key points in the pedestrian system.

Character Area 3 – *Bylaw 9121*
2015/06/15
The High Street Map



	Mixed Use (hotel, office, retail, institutional)
	Housing (Multi-Family Medium Density)
	Mixed Use: • a mix of low to medium density residential with low to medium density residential over retail or live/work uses.
	Mixed Use Employment-Residential
	The High Street
	Open Space System
	Gateway Intersection
	Traffic Calming

Bylaw 8505
2009/07/20



Pedestrian-oriented

Bylaw 9164
2015/03/09

8.2.3 CHARACTER AREA 3 — THE HIGH STREET

The High Street character area occupies the area shown in Character Area 3 – The High Street Map. The High Street is the symbolic heart of the Alexandra neighbourhood, providing a variety of public and places for people to shop, work and socialize. With care and attention to building design, road cross-sections, sidewalks and landscaping, this area will be a social hub and gathering place, catering to the day-to-day needs of area residents and workers.

As part of the character area’s development, the City requires that there be a clearly demonstrated unified approach to land planning, landscaping, buildings, signage, and pedestrian systems. Alexandra Way will a key element of planning in this area.

From a site planning and design perspective, the High Street character area should:

- Offer a mix of uses and public spaces that draw people to shop, eat, socialize, live and work along the street;
- Be lined by uses that attract pedestrians and generate activity throughout the day and evening, everyday;
- Have some areas of curbside parking, pedestrian friendly sidewalks and safe pedestrian crossings.

Land Uses

- Retail and retail services commercial — small floor plate only.
- Office and service commercial.
- Restaurants and neighbourhood pub.
- Entertainment venues.
- Within the area designated Mixed Use, the area abutting the High Street is to be developed as medium density residential over small floor plate retail, and the area not abutting the High Street is to be developed for medium density residential use.
- Within the area designated Residential Mixed Use, a mix of low to medium density residential with low to medium density residential over retail or live/work uses. Live/work uses would include an occupation or profession carried out by an occupant and up to one non-resident employee that is conducted in a mixed commercial/residential unit, the commercial portion of which is clearly designated as being at grade with living space above. From the building exterior, it should present an attractive mixed use image (e.g., retail display windows at grade with residential above) and the work use should be situated at grade fronting the street.

Floor Area Ratio

- See adjacent character areas.

Floorplate: Retail

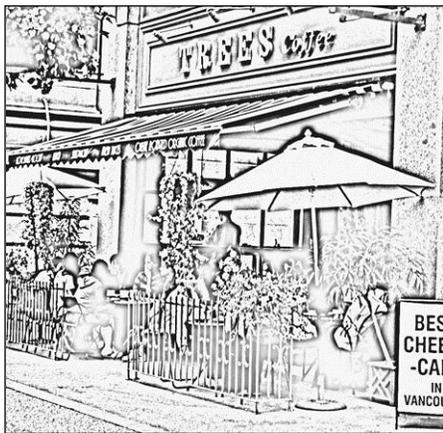
- See adjacent character areas.

Height

- Buildings should be between two and three storeys (8 m to 12 m) at the street.

Frontage

- Building frontages should be limited to 45 m (147.6 ft.); suggest structural bays at 5 to 7 m (16.4 ft to 22.9 ft.).



Streetfront Cafe

Build-to Lines and Finished Grades

- Build-to line: No more than 3 m (9.8 ft.) for the first two storeys, with the exception of building entrances.
- Finished building grades should preferably be level with the sidewalk no more than 0.5 m (1.6 ft.) above or below the curb level of the High Street, and ensure ease of access for people with mobility challenges.

Site Coverage

- See adjacent character areas.

Parking and Access

- On street parallel parking will be accommodated.

Additional Site and Landscaping Considerations

- Consideration should be given to extending landscaping/retail display/outdoor eating into the setback right-of-way; confirm ongoing care through maintenance and liability agreements.

Additional Building Design Considerations

- Small retail shops/businesses are friendly and inviting. Buildings should have architectural features and patterns that provide visual interest, particularly at the scale of the pedestrian.
- Building facades should include a repeating pattern of at least three elements to provide a subliminal rhythm to the street; these elements may include colour, texture, materials, verticality of structural bays, canopies, offsets, and projections, among others.
- Traditional horizontal design details such as building bases, lintels, sills, cornices, and roof lines should be carefully considered and incorporated.
- Windows should be recessed and should include visually prominent sills, shutters, or other such forms of framing to reinforce authenticity.



Accommodate Public Transport



Façade Variety

- Transparency and translucence are essential at pedestrian eye level. Ideally, transparent/translucent windows and door systems should occupy 50% to 60% of a building at grade level along the High Street, generally between 0.5 m (1.6 ft.) and 3 m (9.8 ft.) above the adjacent sidewalk. Ideally, these should be planned for clear viewing or display, rather than for posters and advertising signage.
- Small retail shops are encouraged. Ideally, widths should not exceed 7.6 m (25 ft.), although variation both smaller and slightly wider may be appropriate, depending on the quality of the design.
- Particular attention should be given to entrances, with a view to helping to animate the High Street as well as providing a distinct identity for shops and businesses.
- Consideration should be given to weather and wind protection for pedestrians that complement building design — awnings, transparent canopies.
- Particular attention should be given to the scale, materials and access to buildings at street corners, the roundabout, and Alexandra Way in order to ensure visual coherency.
- Building signage should be incorporated into the building/facade design.
- Lighting should enhance the variation of architectural elements, showcase display windows and encourage visibility into retail/business uses that are open during low light seasons and night. Ambient light from the interiors should consider the safety and enjoyment of the public realm.

Character Area 4 – *Bylaw 9164*
 2015/03/09
 Medium Density Housing Map



8.2.4 CHARACTER AREA 4 — MEDIUM DENSITY HOUSING

This character area occupies the central area of Alexandra (see Character Area 4 – Medium Density Housing Map). If developed to its full potential, this area could accommodate approximately 3,600 housing units and be home to over 6,000. As the predominant built forms will be street-oriented townhouses and apartments, it is likely that the area will appeal to singles, couples and families with older children.

There may be several landowners or developers in this area. The City requires that there be a clearly demonstrated unified approach to land planning, landscaping, buildings, signage, and pedestrian systems. Alexandra Way will be a key element of planning in this character area.

Minimum Lot Area

- 1.0 ha (2.47 ac.).
- No orphan properties of 1 ha or less (2.47 ac. or less), in order to facilitate development as anticipated in this plan and not to perpetuate non-conforming uses (e.g. single detached homes).

Land Uses

- Multi-family housing, with ancillary amenities and community facilities. This may include townhouses and apartments.
- *Bylaw 8505*
2009/07/20
↑ Within the area designated Mixed Use, the area abutting the High Street is to be developed as medium density residential over small floor plate retail, and the area not abutting the High Street is to be developed for medium density residential use.
↓
- Care facility; congregate care facility.
- *Bylaw 9164*
2015/03/09
↑ Within the area designated Residential Mixed Use, a mix of low to medium density residential with low to medium density residential over retail or live/work uses. Live/work uses would include an occupation or profession carried out by an occupant and up to one non-resident employee that is conducted in a mixed commercial/residential unit, the commercial portion of which is clearly designated as being at grade with living space above. From the building exterior, it should present an attractive mixed use image (e.g., retail display windows at grade with residential above) and the work use should be situated at grade fronting the street.
↓

Floor Area Ratio

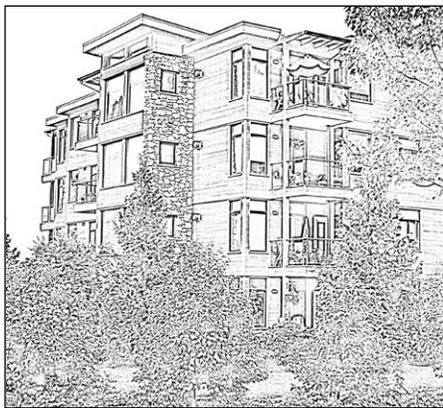
- The total building area, excluding underground parking, will have a base Floor Area Ratio between 1.25 (housing over small plate retail along the High Street) to 1.5 throughout the majority of the area (addition FAR 0.25 to 0.20, respectively, bonus density for affordable housing).

Height

Bylaw 9021
2013/10/15



- Building heights should be primarily between two and four (five and six storeys may be considered for combustible, non-combustible or concrete construction, increased open space, and no additional overshadowing of neighbouring properties); no single storey or tower developments are appropriate.



Variety of Building Materials

Frontage

- Limit building frontage to 45 m (147.6 ft.); suggest bays at 6 m to 8 m (19.6 ft. to 26.2 ft.).

Build-to Lines and Setbacks

- Generally, buildings facing public streets should be 4 m (13.1 ft.) to 6 m (20 ft.) behind the front property line on roads, with the exception of building entrances which may be greater.
- Setbacks for other property lines are flexible and depend on the uses and design in relation to neighbouring buildings and Alexandra Way.

Site Coverage

- Site coverage should be in the range of 40%, and may vary upwards depending on site planning/building massing.

Parking and Access

- At least 70% of vehicle parking should be underground or in-structure and not visible from Alexandra Way.
- Vehicle parking should be kept to a minimum that is considered to be viable for the primary use.
- Provision should be made for secure and accessible bicycle, moped and scooter parking.
- Tandem parking shall be permitted for residents only.
- Encourage shared parking lots to reduce overall parking and to integrate adjacent uses. Consider achieving this through reciprocal operating agreements among owners.
- No private access and driveways are permitted onto the perimeter arterial roads.
- On site loading will be required for all developments.



Wide Sidewalks



Orientation to Alexandra Way

Additional Site and Landscaping Considerations

- Linked courtyards associated with buildings are encouraged as a site amenity. With attention to design/materials, these courtyards will minimize traffic noise, including aircraft noise.
- There should be a continuous, publicly accessible internal pedestrian system that visually and functionally connects each development and leads to Alexandra Way. The appropriate width, materials, configuration and landscaping will be considered during design development.
- Bus stops and drop-off/pick-up points should be considered; where these are included they should be anchored by special design and landscaping.
- The use of mature plant materials will be required in surface parking areas and along vehicle circulation lanes with a view to creating significant landscaping during the early tenure of the developments.

Additional Building Design Considerations

- A mix of townhouses (with street entry) and apartments in each development should be considered.
- Building facades should be articulated to provide visual interest consistent with Alexandra's proposed identity.
- Long blank walls are not acceptable; facade articulation and varied wall materials will be required, particularly on those facades that face the internal pedestrian environment and Alexandra Way.
- Design teams are encouraged to pay particular attention to the scale, materials and access to buildings at intersections to ensure their compatibility with the pedestrian environment.
- Windows and doors should face roads for direct pedestrian access and “eyes on the street”.
- Particular attention will be required along the building's frontage(s) — animating design features should be integrated along the façade.
- Large expanses of reflective and one-material roofs are not acceptable; roof line treatments may vary but should be designed to be compatible with previously constructed, adjacent buildings.
- Building entrances will orient visitors and be aesthetically pleasing. Buildings should have clearly defined, highly visible entrances.
- The inclusion of weather protection measures is encouraged, particularly at building entrances, at key points in the pedestrian system, and at transit stops.



Natural Landscaping Between Buildings

Character Area 5 – *Bylaw 8945*
 2012/10/15
 Low Density Housing Map



- Lighting for the site and buildings should be expressed in a simple, understated manner. Particular attention must be given to lighting for safety/security in pedestrian areas.
- Sustainability features of building design may be expressed in many ways, including: a living roof or any strategy that assists in storm water management, enhanced biomass and energy efficiency; energy systems that result in a minimum of greenhouse gas emissions; efficient building mechanical systems.
- High quality screening of loading areas and utility facilities will be required.
- Mechanical systems and equipment, including elevator penthouses, must be incorporated into architectural roof treatments or effective screening with materials/finishes that are compatible with the building must be provided.

8.2.5 CHARACTER AREA 5 — LOW DENSITY HOUSING

This character area occupies the area towards the eastern portion of Alexandra (see Character Area 5 – Low Density Housing Map). The intended uses in this area is ground-oriented housing – townhouses. This area will be well suited to families, with easy access to the nearby park system and Tomsett Elementary School.

There may be several landowners or developers in this area. The City requires that there be a clearly demonstrated unified approach to land planning, landscaping, buildings, and pedestrian systems.

Minimum Lot Area

- 1.0 ha (2.47 ac.), except that in the townhouse area north of Odlin Road, the minimum lot area shall be 0.5 ha (1.24 ac.) with no permanent access to No. 4 Road permitted (other than emergency vehicle access).
- No orphan properties of 1 ha or less (2.47 ac. or less), in order to facilitate development as anticipated in this plan and not to perpetuate non-conforming uses (e.g. single-family detached homes); except that in the townhouse area north of Odlin Road, the minimum orphan lot area shall be 0.5 ha (1.24 ac.).

Land Uses

- Multi-family housing – townhouses.



Wide Sidewalks

Floor Area Ratio

- The total building area, excluding underground parking, should be no greater than 0.65 (additional FAR 0.1 bonus density for affordable housing).

Height

- Building heights will vary between two and three storeys (8 m to 12 m).

Build-to Lines and Setbacks

- A set back of 5 m (16.4 ft.) from adjacent streets is recommended.

Site Coverage

- Generally 40%, up to 50% may be acceptable, depending on site and internal amenities.

Parking and Access

- At least 60% of vehicle parking should be underground or in-structure.
- Vehicle parking should be kept to a minimum that is considered to be viable for the primary use.
- Tandem parking shall be permitted for residents only.
- Provision should be made for secure and accessible bicycle, moped and scooter parking.
- Particular attention should be paid to designing a safe internal circulation that allows for children's play as well as vehicle access.
- No private access and driveways are permitted onto the perimeter arterial roads (No. 4 Road).
- On site loading will be required for all developments; however, for smaller-scale residential developments, on-street loading may be considered.

Additional Site and Landscaping Considerations

- This character area is intended for town house developments that are suited to families with young children. Design teams should pay close attention to all aspects of site planning and landscaping as they relate to the day-to-day activities of parents with babies, toddlers and young children.
- Play spaces and walkways should be identified first, then buildings incorporated into spaces.
- Special attention should be given to ensuring play areas are observable, and that there are "safe routes to school" and safe/convenient connections among projects (internal and along City roads).



Character Landscaping

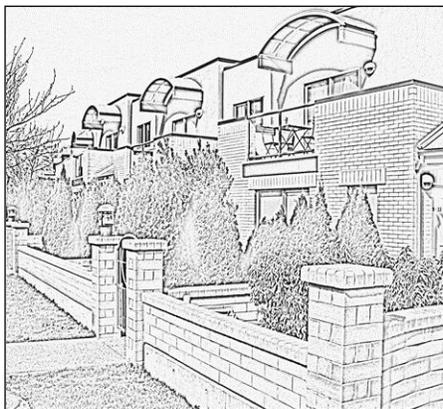
Bylaw 8945
2012/10/15



Internal Streets Useable for Children's Play and Activities

- In light of this area's location near the City's active park and linking greenways, special attention should be given as to how to best integrate with these elements of the open space system.
- Where buildings will be in close proximity to the environmentally sensitive areas, special consideration should be given to potential impacts on wildlife habitat, including storm water run-off and vehicle circulation.
- A survey of existing trees and vegetation should be conducted following the City's information bulletins on tree surveys and protection of trees during construction.
- Where possible and practical, landscaping in this area should be planned and integrated in keeping with the predominant natural environment.

Building Design Considerations



Private Outdoor Space

- Building design should be varied for developments of 12 or more units, while allowing for some commonality of architectural elements.
- The apparent length of buildings should be varied by smaller components that express strong unit identity and provide direct grade access.
- The apparent height of buildings should be de-emphasized, particularly at end walls, through various architectural treatments.
- Front porches and small recessed balconies above grade are encouraged.
- Windows and front doors should add to the sense of neighbourliness, safety and security by providing visual interest and "eyes on the street".

8.3 ALEXANDRA'S LIVABILITY GUIDELINES

Richmond places an emphasis on sustainability and livability for all aspects of planning, development, servicing, and operations. In addition to high standard individual developments, there are other elements that should be addressed by future development for the long-term benefit of those who own, live, work, play, and visit in the neighbourhood.

Adequate, well-designed and integrated social infrastructure is a critical element of a "complete and balanced" community, contributing to residents' overall well being and livability. All aspects of social infrastructure should be fully considered at the time of any rezoning and development permit approval.

8.3.1 AFFORDABLE, RENTAL AND SPECIAL NEEDS HOUSING

At the time of a rezoning for housing, an applicant will indicate how the proposed development complies with Richmond's policy with respect to affordable, rental and special needs housing. In addition to this, the City believes that it is desirable to encourage that, over time, at least 150 units of affordable/special needs housing are built in Alexandra.

8.3.2 PUBLIC SCHOOLS

School District 38 (Richmond) establishes community interests under the School Sites Acquisition Act. The Richmond School District has one elementary school — Tomsett — in Alexandra, located in the north-east part of the neighbourhood. The school has experienced a fluctuation in enrolment during the past 10 years. This site is adequate future population growth within the neighbourhood.

8.3.3 CHILDCARE

The City of Richmond's Child Care Policy is embedded in the OCP. Overall, the City is concerned that families have access to affordable, flexible, high-quality, regulated childcare. At the time of rezoning, an applicant will need to indicate how the project responds to the City's policies. This may lead to negotiations for the provision of City-owned childcare space within private development, or contributions to the City's Child Care Statutory Reserve Budget.

8.3.4 SITE PLANNING

Alexandra's future livability is highly dependent on thoughtful, quality site planning and the relationship/contribution of each project to the neighbourhood as a whole. The City has high expectations of design teams in addressing the following matters.

8.3.5 PUBLIC REALM BEAUTIFICATION — ALEXANDRA BEAUTIFICATION

The City will undertake a beautification program to ensure a high quality public realm in Alexandra including High Street public plazas, street, and street furniture standards.

8.3.6 BARRIER-FREE ACCESS

Design teams should ensure that site and building plans consider the needs for people with mobility, visual, and aural challenges and plan accordingly. Barrier-free connections to adjacent properties, transit stops and the open space system are important.

8.3.7 LIGHTING, SIGNAGE

Design teams are encouraged to use project lighting, signage and other wayfinding devices that are urban in style and scale, and complementary to one another, while remaining consistent with the City's overall policies. Special attention should be given to safety and signage during nighttime and seasonally low-light conditions.

8.3.8 CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED)

Preventing crime is a concern to all people. Design teams should take into consideration the principles of CPTED (Crime Prevention Through Environmental Design) for all projects. Projects that abut Alexandra Way must clearly demonstrate ways to assist in enhancing security for people and prevent vandalism.

8.3.9 RELATIONSHIP WITH OPEN SPACE SYSTEM

Importantly, design teams for private developments will demonstrate how they have taken into account the impact of any proposed development on the public realm as a whole and how the proposed development contributes to the neighbourhood's open space system. Designers who are involved in publicly funded projects, such as roads and parks, will also demonstrate how an individual project complements the overall open space system.

8.3.10 TREE RETENTION AND REPLACEMENT

Developers are required to meet City tree bylaws, policies and guidelines. Developers are encouraged to retain, protect, and plant trees and vegetation in conjunction with their projects.

8.3.11 PUBLIC ART

The City is committed to public art and has a “Percent for Public Art Policy” with the view that new development can enhance and humanize the public realm by providing high-quality art in publicly accessible spaces; and create exciting, harmonious environments. Developers and their design teams are encouraged to voluntarily incorporate artwork into building exteriors and landscaping in a way that contributes to the public realm.

8.3.12 HERITAGE

The City's OCP contains policies related to heritage (see Section 6.7). In Richmond, heritage refers to anything of a physical, cultural, or social nature that is unique to and valued by a community, and can be passed from generation to generation. In Alexandra, three houses and four trees are identified as heritage. If these resources are potentially affected by a proposed development, developers will be encouraged to conserve the resource in their original context or, if this is impossible, to determine whether relocation, partial-preservation or other form of compensation is possible.

8.3.13 IMPACT AND CUMULATIVE EFFECTS STUDIES

At the time of rezoning, the City may ask the proponent to undertake various impact or cumulative effects studies at their expense, including the identification of mitigations, if any are warranted. If this is required, the City will establish the terms of reference of this study in consultation with the applicant. The study scope will vary depending on the nature of the project, but may include such items as economic, traffic and noise impacts, micro climatic change, and impact on policing and security.

8.4 ALEXANDRA'S BUILDING SUSTAINABILITY GUIDELINES

All developments will be encouraged to take into account the following practices in the interests of long-term environmental, financial and social sustainability.

8.4.1 ENERGY AND WATER USE

Design teams will be required to identify ways in which they are accomplishing optimal energy (e.g. heat, light, etc.) and water use in site planning and building design.

8.4.2 STORMWATER MANAGEMENT

As feasible, major new developments should include on-site facilities to manage storm water, and to ensure it does not produce a net income in storm water flows. Design teams are encouraged to minimize impervious surfaces at grade and examine opportunities to minimize impervious roof surfaces.

8.4.3 SOLID WASTE MANAGEMENT

Design teams should ensure that recycling opportunities are designed into exterior and interior spaces.

8.4.4 CONSTRUCTION WASTE MANAGEMENT

The salvaging and recycling of all site materials is encouraged during the redevelopment process. During construction, appropriate erosion and sediment control practices will be required.

8.4.5 ALEXANDRA DISTRICT ENERGY UNIT

Bylaw 8715
2011/03/21



To reduce greenhouse gas emissions, as indicated in Table 1 below, the following 5 development proposals (e.g., in-stream proposals) may receive an additional 4% Density Bonus upon committing the project to hook into the Alexandra District Energy Unit.

<i>Bylaw 8715 2011/03/21</i>				
Table 1 – Additional ADEU Density Bonus for Certain Instream Applications				
In Process	Address	Approved/ Proposed FAR	Additional Square Footage	New Total FAR
RESIDENTIAL				
GBL Architects	9388 Odlin Road (formerly 9340, 9360 and 9400 Odlin Road)	1.7	7,624	1.76
0797460 BC Ltd.	9393 Alexandra Road	1.6	4,627	1.66
Polygon – Mayfair Place	9331, 9351, 9371, 9391 and 9411 Odlin Road	1.68	12,275	1.74
Fred Adab Architecture	9251 and 9291 Alexandra Road	1.5	4,095	1.55
COMMERCIAL				
Smart Centres	4660, 4680, 4700, 4720 and 4740 Garden City Road; 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480 and 9500 Alexandra Road	0.61	3,026	0.615

9.0 IMPLEMENTATION

9.1 VIABLE REDEVELOPMENT

ISSUE:

Redevelopment of the Alexandra sub-area will be undertaken primarily through private landowners and developers. In order to achieve the vision for Alexandra set out in this plan, it is necessary to establish minimum parcel sizes to facilitate land assemblies and prevent the “orphaning” of individual properties.

OBJECTIVE 1:

Establish viable land parcels for redevelopment of Alexandra for urban uses.

POLICIES:

- a) The minimum parcel size to facilitate Alexandra’s redevelopment varies among the five character areas (see Section 8.2).

9.2 EFFECTIVE IMPLEMENTATION

ISSUE:

The redevelopment of the Alexandra sub-area will be complex. As of 2006, there were over 140 landowners. It is imperative that the City and landowners have a clear and viable implementation strategy to facilitate redevelopment as envisaged in this plan.

OBJECTIVE 1:

Coordinate and facilitate the redevelopment of Alexandra through an effective implementation program.

POLICIES:

- a) Landowners and developers will work with the City to accomplish the redevelopment of Alexandra as set out in the Alexandra Neighbourhood Implementation Strategy (Section 9.3).

9.3 ALEXANDRA NEIGHBOURHOOD IMPLEMENTATION STRATEGY

9.3.1 PURPOSE

The Alexandra Neighbourhood Implementation Strategy is an integral component of the West Cambie Area Plan Bylaw.

The West Cambie Area Plan establishes the City's vision, goals, objectives and policies for West Cambie and the Alexandra Neighbourhood within a legal framework. It provides a decision-making blueprint to guide and manage development and change in the Alexandra Neighbourhood. Once adopted, the Plan Bylaw may only be changed through a Bylaw amendment process that includes public notification to affected area residents about the proposed changes, and a Public Hearing to allow residents an opportunity to present their views, prior to City Council making a decision.

The Alexandra Neighbourhood Implementation Strategy identifies City and developer roles, mechanisms, partnerships and resources to achieve the adopted Bylaw directives. The Strategy is adopted by Council as a component of the Bylaw. Once adopted, the Strategy may only be updated through the Bylaw amendment process, as outlined above, as required to coincide with City initiatives, such as servicing targets, capital works development, budgets and other matters.

9.3.2 ALEXANDRA DEVELOPMENT FRAMEWORK

ISSUE:

The Alexandra Neighbourhood, approximately 60 ha (150 ac.) in size, is intended to support a new mixed-use community in the West Cambie Area of Richmond. The existing neighbourhood is predominantly greenfield in character. Most land is zoned single-family residential, and is minimally serviced (existing open drainage ditches, water lines for current low density uses, and septic fields). There are several greenhouse operations. Road access is limited to two east-west roads, without sidewalks and boulevards. Most existing sites are expected to be developed for new land uses (other than the Tomsett Elementary School, local convenience commercial, and gas station uses) and new services are required.

OBJECTIVE 1:

Follow a development-led approach to provide roads, services, infrastructure, and amenities to support the development of the Alexandra neighbourhood.

POLICIES:

- a) Development is responsible for financing the required works and services;
- b) Require all major off-site upgrades to be in place prior to issuance of any occupancy permits.

ISSUE:

The redevelopment of Alexandra will require a range of financing tools in order to achieve timely growth in a fair and transparent process.

OBJECTIVE 2:

Payment for development costs must be structured so as not to impose too high a risk to developers or to jeopardize the feasibility of development. Established rates provide predictability and certainty for the development community.

POLICIES:

- a) The City will not borrow to front-end infrastructure costs;
- b) The City will work with the development community to consider a range of tools permitted under the Local Government Act for development financing;

On-Site (Internal) Improvements

- c) Developers will be required to construct all necessary works and services to a local standard within the Alexandra Neighbourhood on their frontages, at their sole cost (subject to Development Cost Charge credits and rebates via Front-End Agreements, where applicable), under a Standard Servicing Agreement, including:
 - All of the local roads (i.e., roadway, curb and gutter, sidewalk, grass and treed boulevard, street lighting, etc.);
 - Traffic calming and intersection improvements;
 - Sanitary sewerage collection systems, storm water collection systems, and water systems, to a local standard;
 - Electrical, telephone, cable, and gas distribution systems.

Off-Site (External) Improvements – Sanitary and Drainage

- d) Developers will be required to construct:
- The Odlin Road sanitary sewer pump station and force main;
 - Upgrades to the Odlinwood, Walford and Kilby sanitary pump stations;
 - Upgrades to the area wide (external) storm drainage system to meet capacity demands.

The financing arrangements for these works and services must be in place prior to rezoning approval, with the actual works and services to be completed under a Servicing Agreement as a condition of occupancy permit approval.

Developers will be eligible for Development Cost Charge credits from the City-wide DCC Program for these works and services to the maximum extent of their Development Cost charge payments. Additional costs for these works and services above the Development Cost Charge credit may be recovered by:

- Private agreement amongst the developers; or
- A Development Works Co-ordinated Agreement with the City.

Transportation Improvements

- e) Developers along Garden City Road, Cambie Road, No. 4 Road, and Alderbridge Way will be required to undertake the necessary traffic improvements, including:
- Traffic signals (Local and City-wide DCC program);
 - Left and right turn bays (Local and City-wide DCC program);
 - Pedestrian signals (Local Area DCC program);
 - A bike lane on Alderbridge Way (City-wide DCC program).

Developers will be eligible for Development Cost Charge credits from either the Local Area DCC Program or the City-wide DCC program for these traffic improvements to the maximum extent of their Development Cost Charge payments to the respective program.

Parks

- f) Developers will not be eligible for any Development Cost Charges credits, unless they dedicate land for parks and construct park improvements, in which case they will get credits from the Local or City-wide DCC Program to the maximum extent of their Development Cost Charge payments.

Local Area DCC Program

- g) Developers will be responsible for oversizing the sanitary sewerage collection system, storm water collection systems and water systems above a local standard where required; and for higher standards of construction for the High Street.

Developers will be eligible for Development Cost Charge credits from the Local Area DCC Program for these works and services to the maximum extent of their Development Cost Charge payments only (no Latecomer Agreements on DCC Program works).

- h) Developers will be responsible for construction of the local roads on their frontages.

Where specified roads have been added to the Local Area DCC Program (e.g., north-south roads in the Alexandra neighbourhood), developers will be eligible for Development Cost Charge credits and rebates via Front-End Agreements from the Local Area DCC Program for the cost of land and the construction of the specified roads, only, to the maximum extent of the cost in the Local Area DCC Program.

ISSUE:

The redevelopment of Alexandra will require a range of amenities to create a high quality and highly livable public realm and to meet the needs of families and households with a range of incomes.

OBJECTIVE 3:

Development needs to contribute financially to achieve public benefits. Establish bylaws, policies and guidelines for developer contributions to implement the Area Plan.

Bylaw 8916
2012/07/16



POLICIES:

Density Bonusing – Affordable Housing

- a) Density Bonusing will be offered to developers where they build affordable housing with their development;
- b) The intent of density bonusing for affordable housing is to secure a number of affordable housing units within a development (e.g., 5% of the total units) and to permit additional density for market housing as a financial incentive to the developer for building the affordable housing;

Bylaw 8916
2012/07/16

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- c) Conceptually, the increased density bonus (DB) will be allocated, as follows:
 - One-third of the DB, for affordable housing; and
 - Two-thirds of the DB to pay for the affordable housing and to provide a developer incentive;
 - Note that this formula may vary slightly, based on an economic analysis during the development application review process;
 - d) City staff and developers will work together to achieve this goal;
 - e) Provide a cash contribution towards affordable housing only in Council approved special development circumstances, while continuing to meet the City's affordable housing policy requirements.

Bylaw 9793
2018/02/19



Developer Contributions – Public Amenities

- f) For rezoning applications for sites depicted on the 'Alexandra Neighbourhood Land Use Map', the City will accept developer/applicant contributions as follows:
 - Affordable Housing: With the exception of the 'Mixed Use Employment Residential Area' designation, where a development does not build affordable housing, contributions of \$65.55 per m² (\$6.09 per ft²) to the Affordable Housing Statutory Reserve Fund will be accepted (and no density bonus for affordable housing will be granted);
 - Child Care: The City will accept a developer's contribution of \$7.75 per m² (\$0.72 per ft²) on the proposed total net floor area (based on the proposed FAR) to assist in paying for child care facilities;
 - City Beautification: The City will accept a developer's contribution of \$7.75 per m² (\$0.72 per ft²) on the proposed total net floor area (based on the proposed FAR) to assist in paying for City beautification works (e.g. "High Street" streetscaping; public realm, walkways, plazas, feature landscaping);
 - Community and Engineering Planning Costs: The City will accept a developer's contribution of \$0.86 per m² (\$0.08 per ft²) on the total net floor area (based on the proposed FAR) to assist in paying for community planning and engineering costs to plan community land use, services and infrastructure. The City may reduce or eliminate this contribution for new rezonings which provide secured market rental housing;

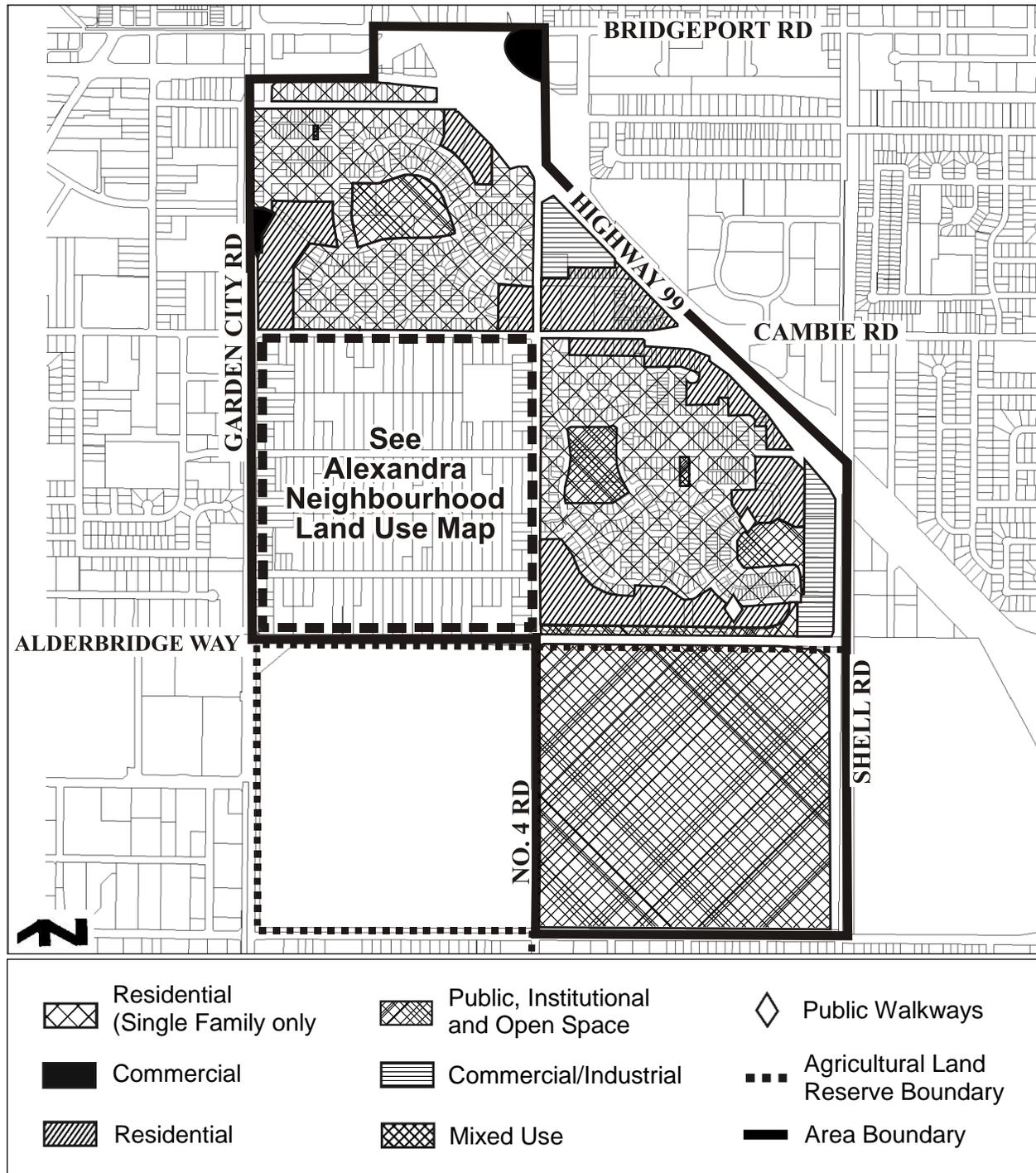
Bylaw 9889
2018/09/04

Bylaw 9793
2018/02/19

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- g) On February 28, 2019, and then by February 28 every two years thereafter, the above contribution rates are to be revised by adding the annual inflation for the preceding two calendar years using the Statistics Canada *Vancouver Construction Cost Index – Institutional* inflation rate for adjusting the above Affordable Housing, Child Care and City Beautification contribution rates; and the Statistics Canada *Vancouver Consumer Price Index – All Items* inflation rate for adjusting the Community and Engineering Planning Costs contributions rates; with revised rates published in a City Bulletin;
 - h) A minimum of 5% of the total residential building area is required in the form of built Affordable Housing units, with an additional 7.5% of the residential floor area being provided in the form of built modest market rental units, and 2.5% of the residential floor area is provided as market rental units that are secured in perpetuity as rental units, as per the West Cambie Alexandra Neighbourhood Mixed Use Employment-Residential Use Density Bonus, Community Amenity Contribution Modest Rental Housing Rates Policy.

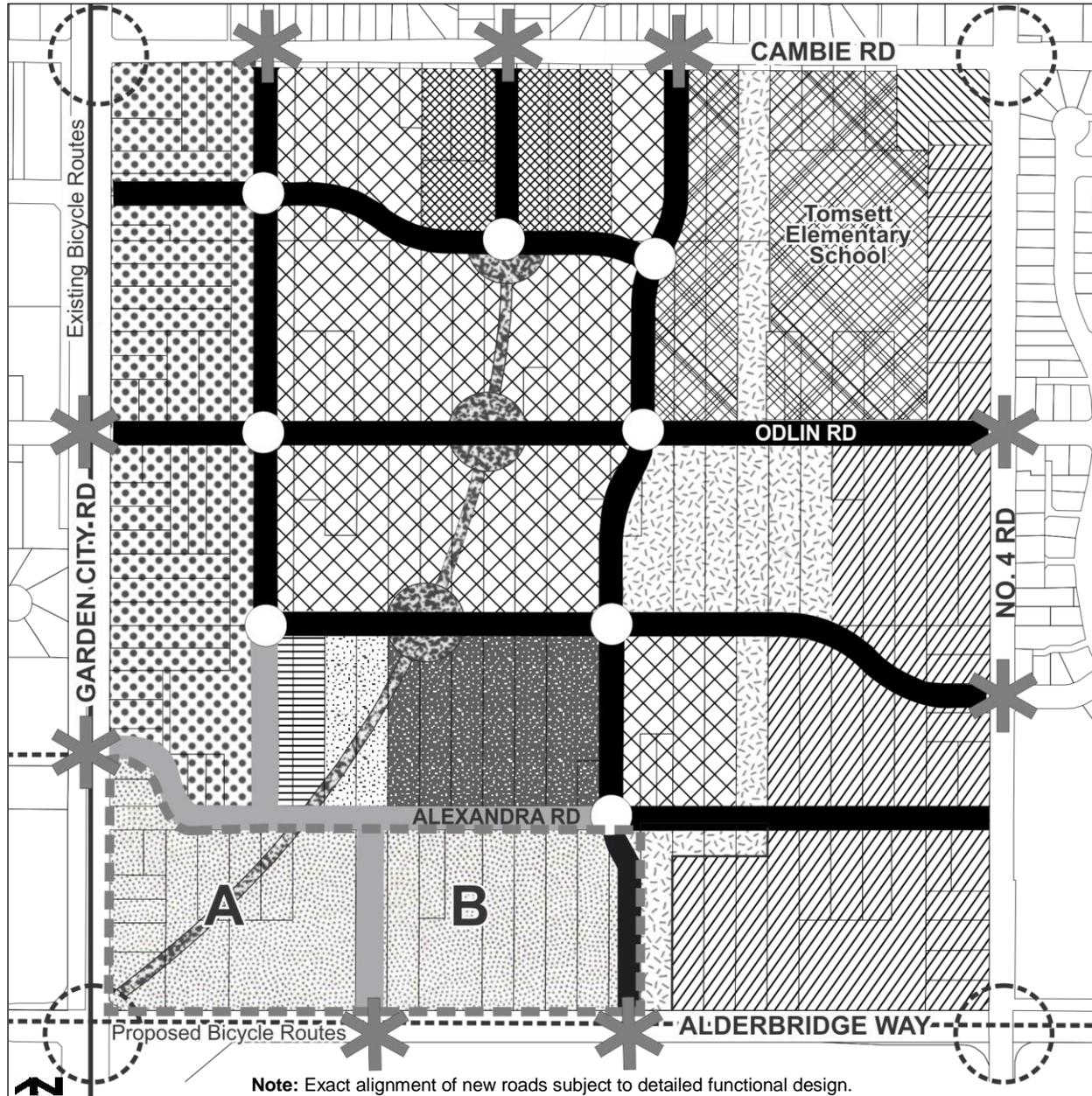
Cash-in-lieu contributions are not acceptable and the affordable housing contributions in Policy f) above will not apply to the Mixed Use Employment-Residential designated lands.

Land Use Map



Alexandra Neighbourhood Land Use Map

Bylaw 9121
2015/06/15



Area of No Housing Affected by Aircraft Noise	Residential Area 2 0.65 base FAR (Max. 0.75 FAR with density bonusing for affordable housing). 2 & 3-storey Townhouses.	Mixed Use: • abutting the High Street, medium density residential over retail; • not abutting the High Street, medium density residential. 1.25 base FAR. Building heights low to mid-rise. (Max. 1.50 FAR with density bonusing for affordable housing)	New Traffic Signals
Mixed Use Employment – Residential (Minimum 0.52 FAR Employment space) - base FAR of 0.52 (Maximum 1.8 FAR, subject to Section 8.2.1.)	Residential Mixed Use • a mix of low to medium density residential with low to medium density residential over retail or live/work uses. Max. 1.25 FAR. Building heights low to mid-rise. (Max. 1.50 FAR with density bonusing for affordable housing).	Community Institutional	Feature Intersections - details to be developed
Convenience Commercial	Mixed Use: Hotel, office and streetfront retail commercial. Area A: Min. 0.60 FAR up to 2.0 Area B: Large and small floor plate up to 1.0 FAR	Park: North Park Way, Central Park, South Park Way	Feature Landmarks in Combination with Traffic Calming Measures
Residential Area 1 1.50 base FAR (Max. 1.70 FAR with density bonusing for affordable housing). Townhouse, low-rise Apts. (4-storey typical)		Alexandra Way (Public Rights of Passage Right-of-way)	
Residential Area 1A 1.50 base FAR (Max. 1.75 FAR with density bonusing for affordable housing). Townhouse, low-rise Apts. (6-storey maximum).		Proposed Roadways	
Residential Area 1B 1.68 base FAR (Max. 1.88 FAR with density bonusing for affordable housing) (6-storey maximum).		High Street	

Also refer to Section 8.4.5 – Alexandra District Energy Unit regarding district energy density bonusing policies.

